

# Fairhaven

## 40R Smart Growth Overlay District

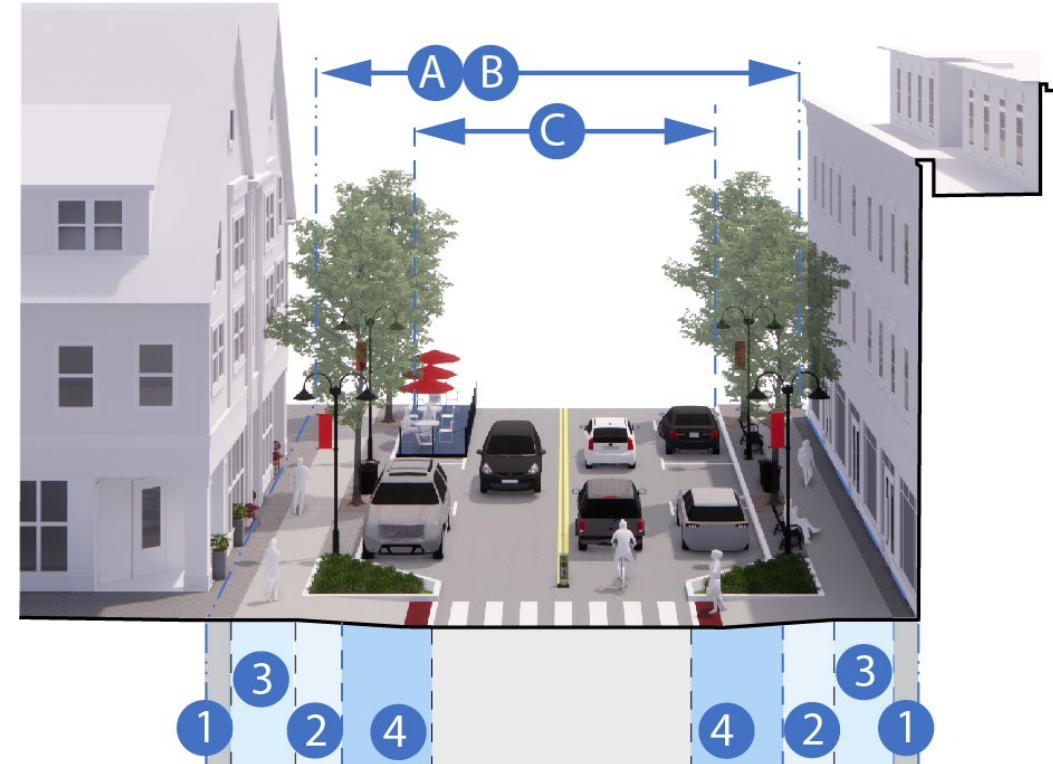
### Pre-Application Public Hearing

June 10, 2024



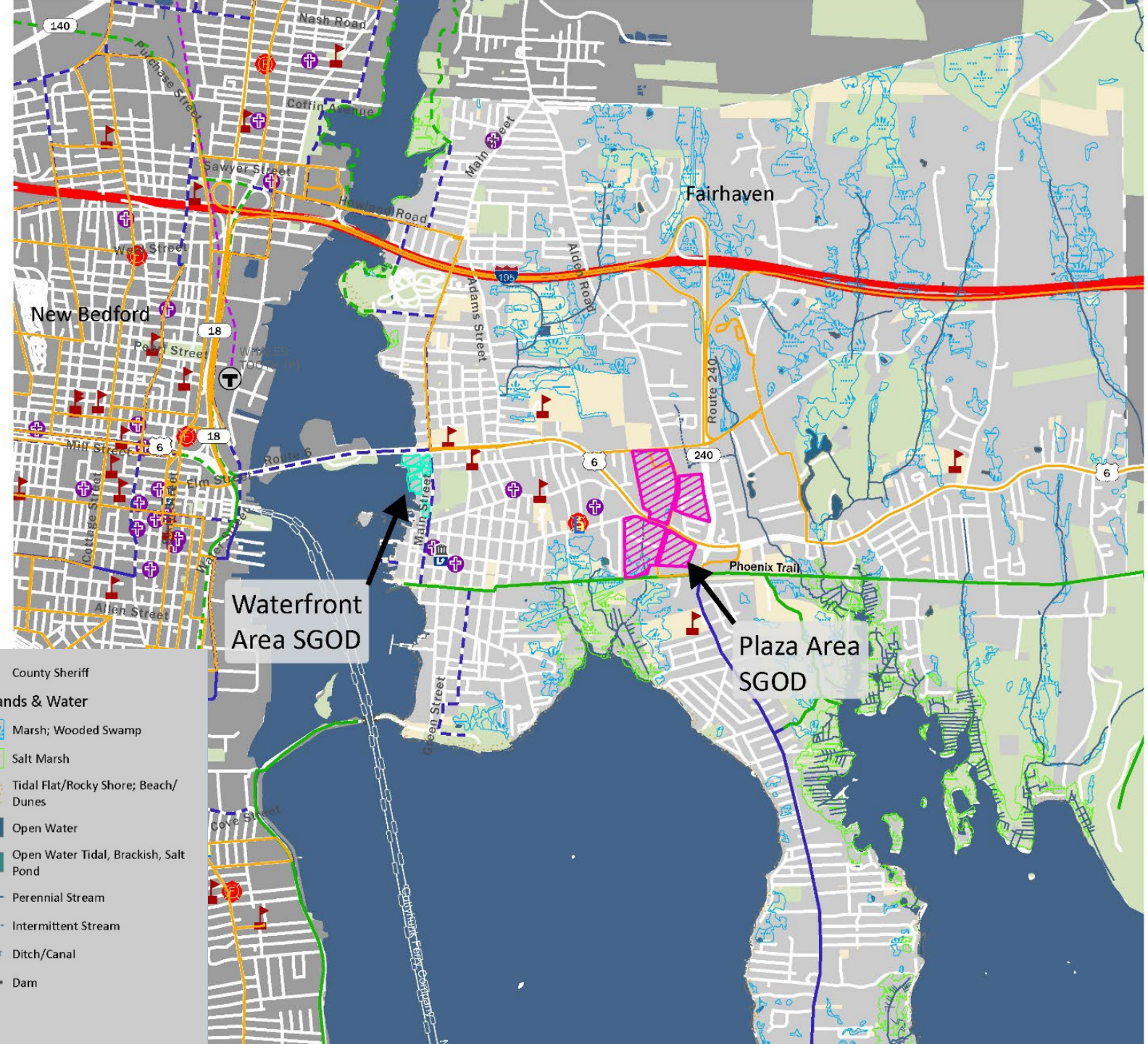
**Fairhaven**  
Massachusetts

**DODSON & FLINKER**  
Landscape Architecture and Planning





# Fairhaven 40R Proposed Smart Growth Overlay District— Waterfront Area and Plaza Area



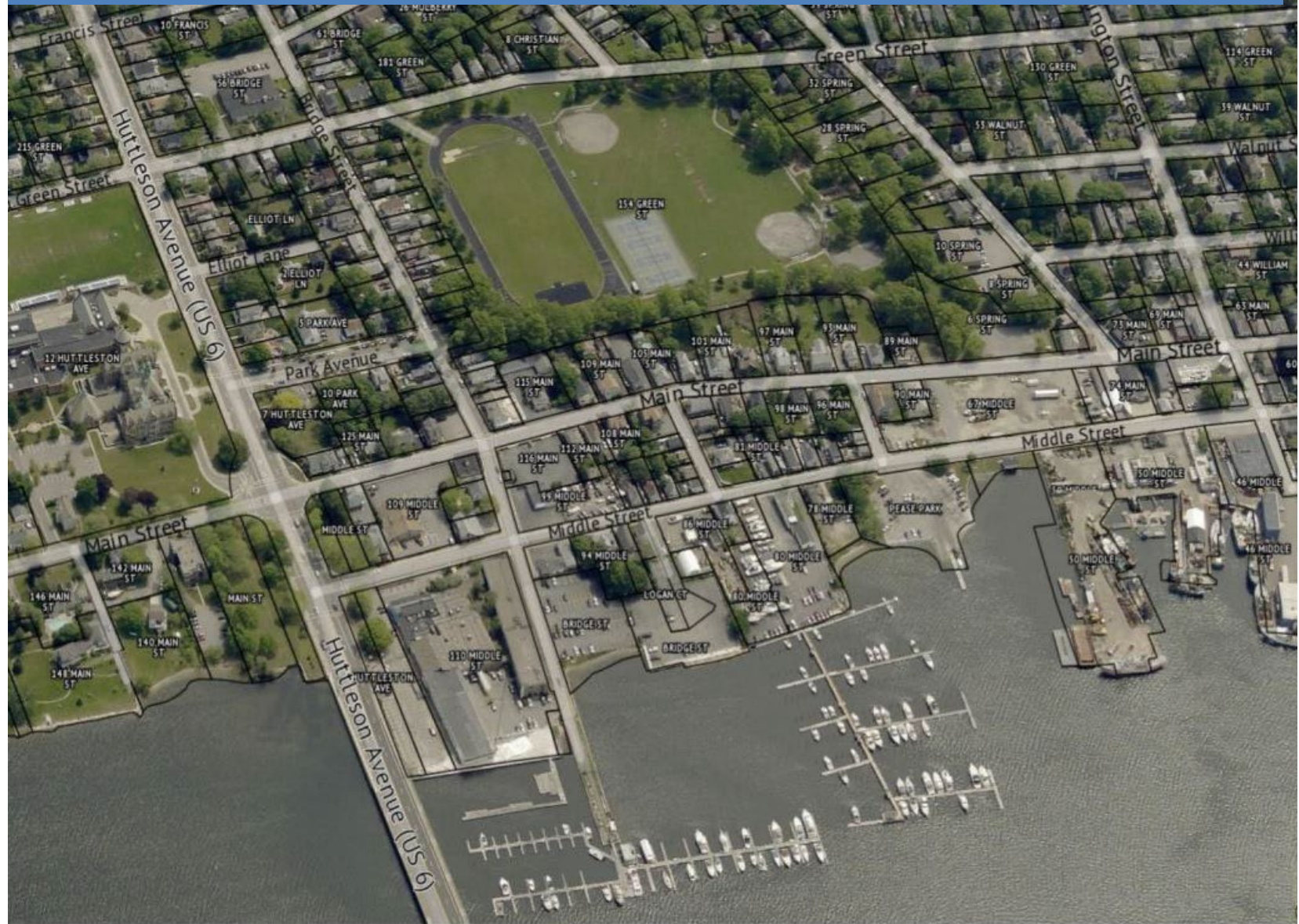


# Plazas Area





# Waterfront-Main/Middle





# Planning Process

## Volunteer Working Group

- September 21, 2022
- October 5, 2022
- November 16, 2022
- December 14, 2022
- January 25, 2023
- December 13, 2023





# Planning Process: Visioning Workshop: November 4-5, 2022





# Fairhaven's Overall Needs from Planning Process

## **Affordable and Attainable housing**

- Local workers
- Young people
- Older adults
- Prepare for Commuter Rail

## **Community Hubs, Activities, Local Businesses**

- Cafes, pubs/breweries, game rooms
- Live music
- Independent bookstores
- Artist galleries
- Movie theater
- Permanent space for Huttleston Craft Market
- Shared workspaces

## **Sustainability & Resilience**

- Long-term planning for sea-level rise
- More trees, less pavement
- Improvements to sewer and public water supply

## **Connectivity**

- More walkability, less car-dependency
- Improved sidewalks, crosswalks, bike infrastructure
- Shuttle to train station

## **Tax Base**

- Need to maintain commercial tax base
- Desire for new growth



# Plazas Area—Takeaways from Planning Process

## Summary of Strengths and Weaknesses

- Good transportation access
- High traffic
- Inadequate bike/ped infrastructure in area, but close to Phoenix Rail Trail
- Underutilized or vacant commercial space & parking
- Proximity to public amenities: Senior Center and schools, bike path, and conservation areas to the south
- Close to town center
- Huge space! Lots of potential
- Powerlines—a challenge and opportunity
- Current Zoning does not allow residential use

## Opportunities

### Economic

- Commercially-zoned area with space for redevelopment
- Trends in commercial real estate favor “experience businesses,” which are compatible with residential uses

### Housing

- Close to Senior Center
- Staples Plaza is adjacent to wetland, bike path, existing residential neighborhood
- Interior of parcels suitable for taller buildings

### Public Realm

- Redevelopment is an opportunity for complete streets improvements
- Cut-through traffic shows desired new roads
- Powerline Corridor—explore bike path corridor, provides space for parking



# Plazas Area





# Existing Conditions





## Final Vision Plan

The vision plan shows one potential outcome from long-term redevelopment in the area.

The vision plan was created by Dodson & Flinker, the Town's consultant for the 40R zoning project, based on public input. It is not a development proposal for an actual project. The kinds of changes shown in the vision plan would be initiated by individual property owners, based on their goals and subject to the requirements of the proposed 40R zoning.

The vision plan is intended to guide future incremental development so that it adds up to a cohesive place. For example, it suggests locations for future streets and parks and it illustrates principles for appropriate development in the 40R area. Elements of the vision plan were also incorporated into the proposed 40R Overlay Zoning Bylaw and Design Standards.







New development fills in empty pad sites at the front of lots and underutilized parking areas

Some underutilized commercial spaces may be replaced with new buildings

↔ Existing driveways and parking lot lanes are converted to streets lined with sidewalks, street trees and attractive buildings

A continuous network of sidewalks connects to buildings and public spaces—enabling people to park once and explore the area on foot, like a traditional downtown





**C** New civic spaces are created in prominent and accessible locations

Landscaping and open spaces are intentionally designed to be usable by people and to beautify the area. They are located along streets and between buildings make the area more interesting.


**P** Off-street parking is conveniently located at the center of blocks, hidden by buildings. It can be shared by adjacent uses.


**P** On street parking is also convenient, especially for quick trips, and buffers sidewalks from travel lanes.

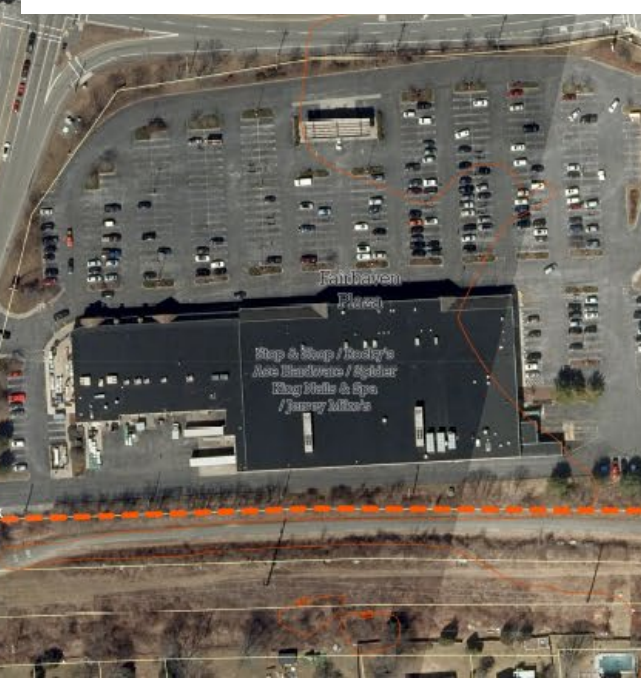




New streets create pedestrian-scale blocks that transform large parking lots into an attractive place to live, work, and play

 Existing residential areas are buffered from new development


  Sidewalks, a shared use path, and/or trail link to the Phoenix Rail Trail





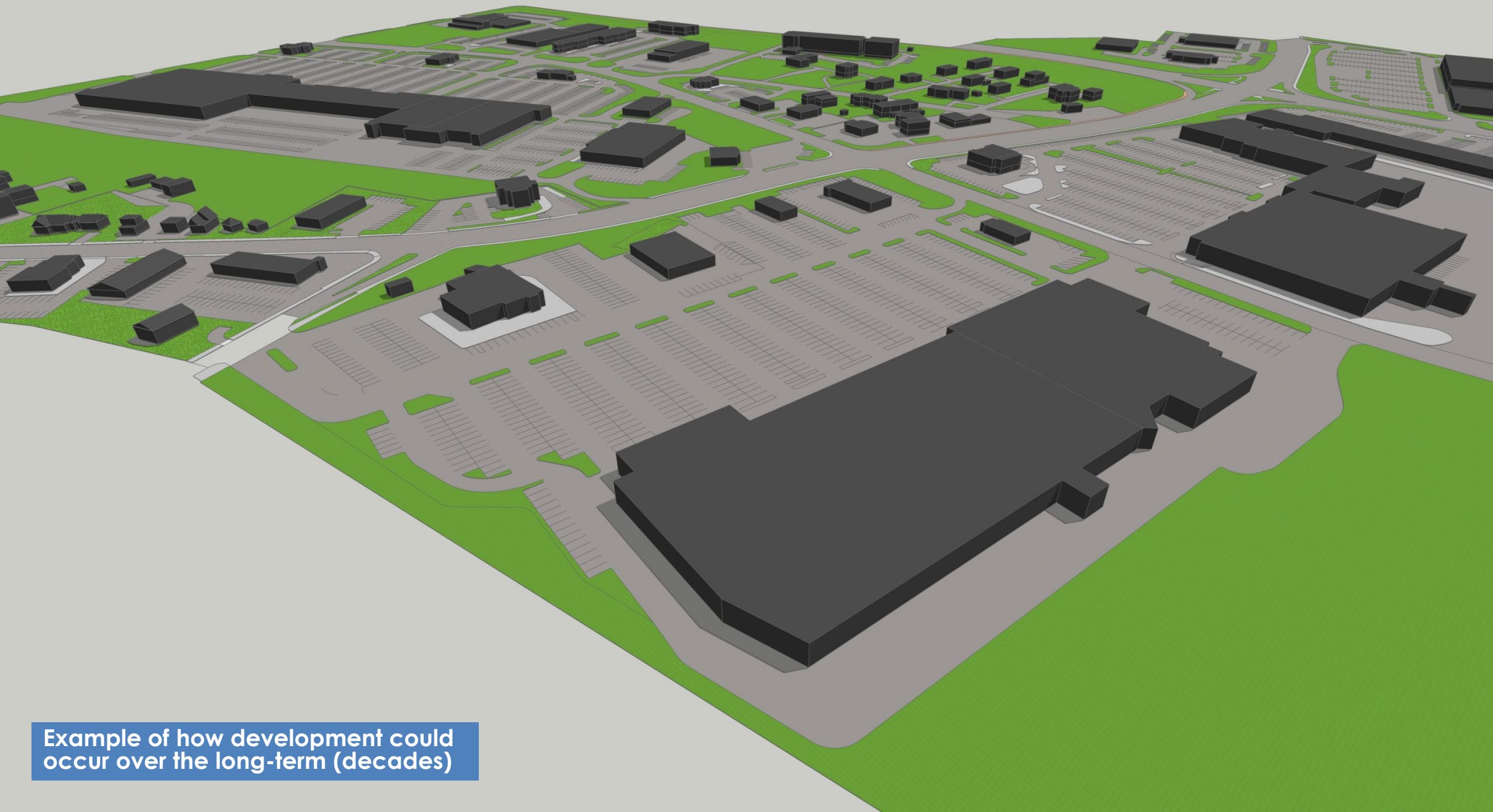


Elizabeth Street

 A new park lined with buildings is a focal point for the southern part of the district. Cafes and other active ground floor spaces spill out onto the sidewalks around the park making a vibrant place to visit.

Buildings are designed to be human-scaled. They are composed of smaller masses with niches and bumpouts that provide visual interest and space for plantings and outdoor seating. Varied roof forms reflect historic buildings in Fairhaven.





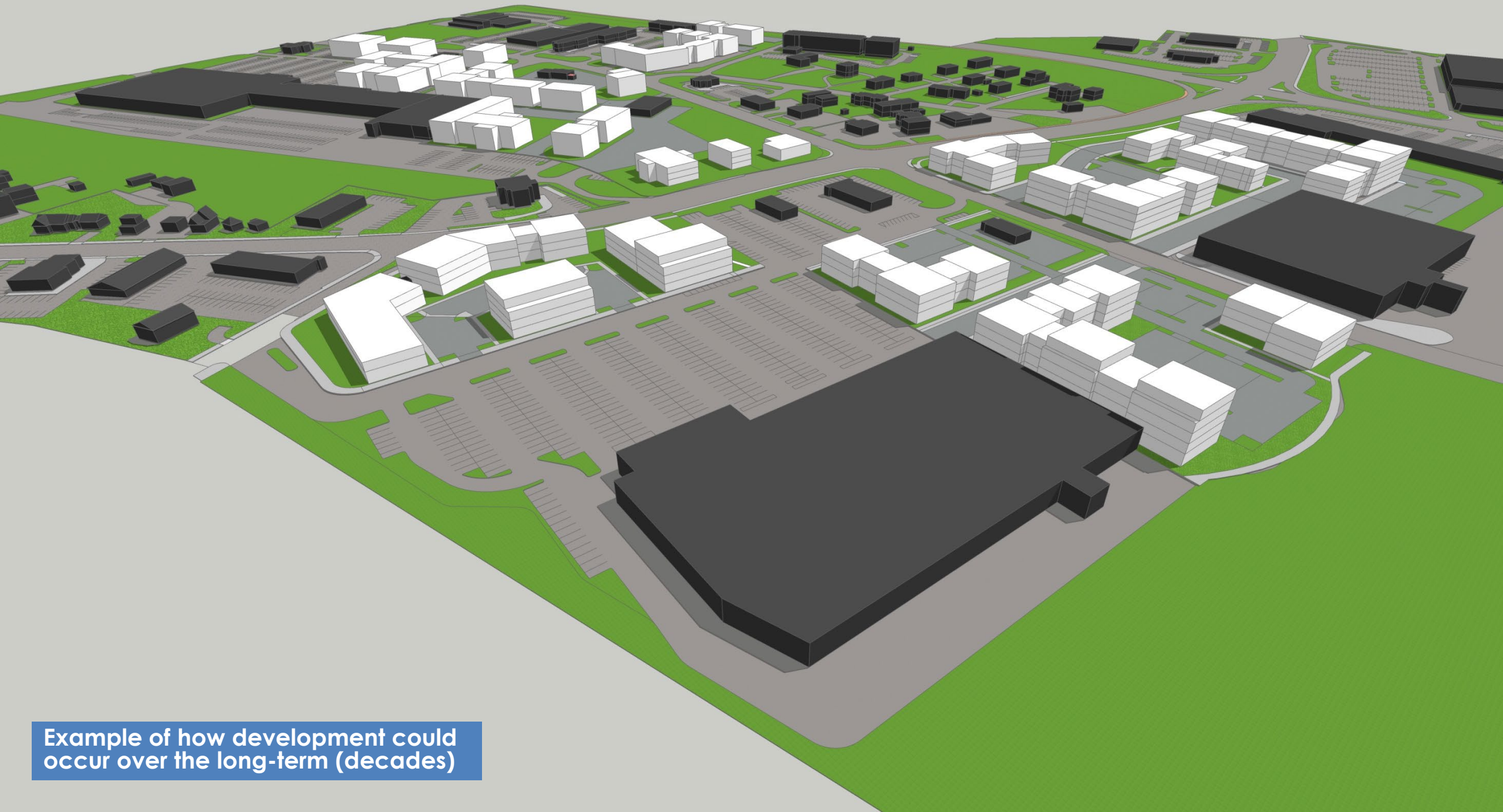
Example of how development could occur over the long-term (decades)





Example of how development could occur over the long-term (decades)





Example of how development could occur over the long-term (decades)





Example of how development could occur over the long-term (decades)







# Waterfront—Takeaways from Planning Process

## Strengths and Weaknesses

- Highly-valued location
- Already has a mix of uses: residential, resort, shipyard, marina, auto-repair, funeral home, VFW, etc.
- Constraints to new development: sea level rise, sewer, high ground water, fill
- Working waterfront
- Historic properties
- Narrow right-of-way along Main & Middle St
- Area is a gateway to Fairhaven
- Closet to town center
- Current zoning requires special permit for multi-family. Parking requirements limit development potential.

## Opportunities

### Economic

- Waterfront attractive location for many uses
- Potential for more waterfront event space
- Close to jobs

### Housing

- Some potential for additional development, but many business properties unlikely to be redeveloped in short-to-medium-term
- Long-term need to elevate for storm surges

### Public Realm

- Desire for more attractive sidewalks, bike facilities, street trees
- Could improve entrance to Cushman Park and connection between park and Pease Park Boat Ramp



# Waterfront Area



The vision plans on the following slides shows potential outcomes from long-term redevelopment in the area.

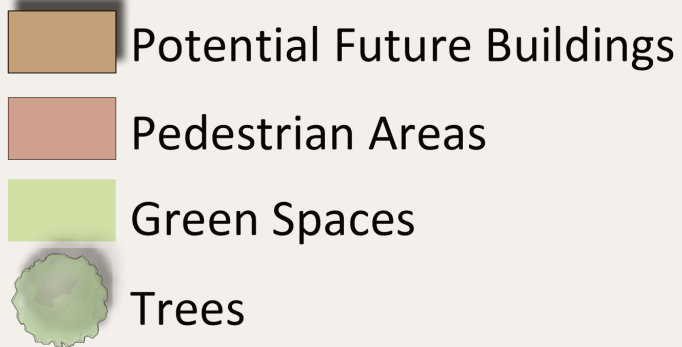
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The vision plans are intended to guide future incremental development so that it adds up to a cohesive place. For example, they suggest locations for future streets and park spaces and illustrate principles for appropriate development in the 40R area. Elements of the vision plans were also incorporated into the proposed 40R Overlay Zoning Bylaw and Design Standards.



# Waterfront Potential 40R District Vision Plan 1

35 units/acre max.  
with surface parking



Like older buildings in the neighborhood, new buildings line up along the edges of streets

There is a mix of building types and sizes:

- a** apartment buildings
- t** townhouses
- f** stacked flats

**P** Surface parking areas are hidden behind new buildings. Parking areas are shared by nearby buildings, where possible.

A continuous network of sidewalks and paths connects all buildings & outdoor spaces

**<...>** A pedestrian path runs along the waterfront



# Waterfront Potential 40R District Vision Plan 2

35 units/acre max.  
with mixed surface  
and ground floor  
parking

- Potential Future Buildings
- Pedestrian Areas
- Green Spaces
- Trees



**P** Ground floor parking within some buildings opens space for a new street and civic spaces. Along street frontages, parking is hidden behind housing or commercial space.

**↔** A new tree-lined street runs parallel to Middle Street. Another extends from Cowen Street. Together they define a new block.

**↔** A path that passes through a new greenspace connects Cushman Park and the Pease Park Boat Ramp

**C** New civic spaces are created in prominent and accessible locations



# Waterfront Potential 40R District Vision Plan 3

Up to 50 units/acre  
(requires density  
bonuses) with mostly  
ground floor parking

- Potential Future Buildings
- Pedestrian Areas
- Green Spaces
- Trees



Additional ground floor parking within buildings enables more space (+) for housing and commercial uses

Ground floor parking also raises the habitable space of buildings above expected future flood levels

The additional development shown in this plan would only be possible with a discretionary waiver from the Plan Approval Authority based on community benefits provided by the applicant.




# Waterfront Potential 40R District Vision Plan 3

Up to 50 units/arce  
(requires density  
bonuses) with mostly  
ground floor parking

- Potential Future Buildings
- Pedestrian Areas
- Green Spaces
- Trees



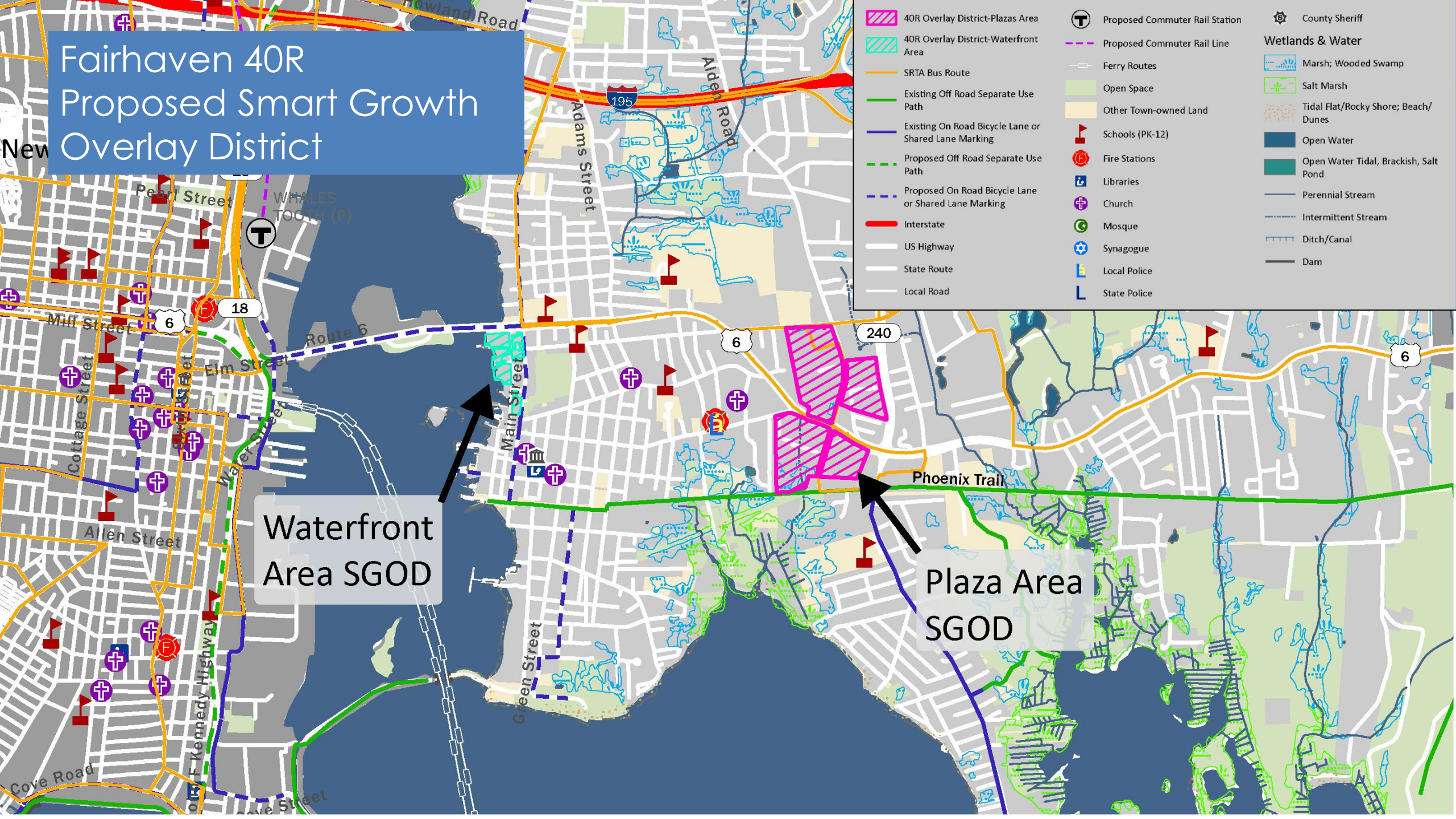
 Proposed overlay district

The proposed overlay district mostly covers properties that are currently used for parking or a commercial use and that are not historic



# Fairhaven 40R Proposed Smart Growth Overlay District

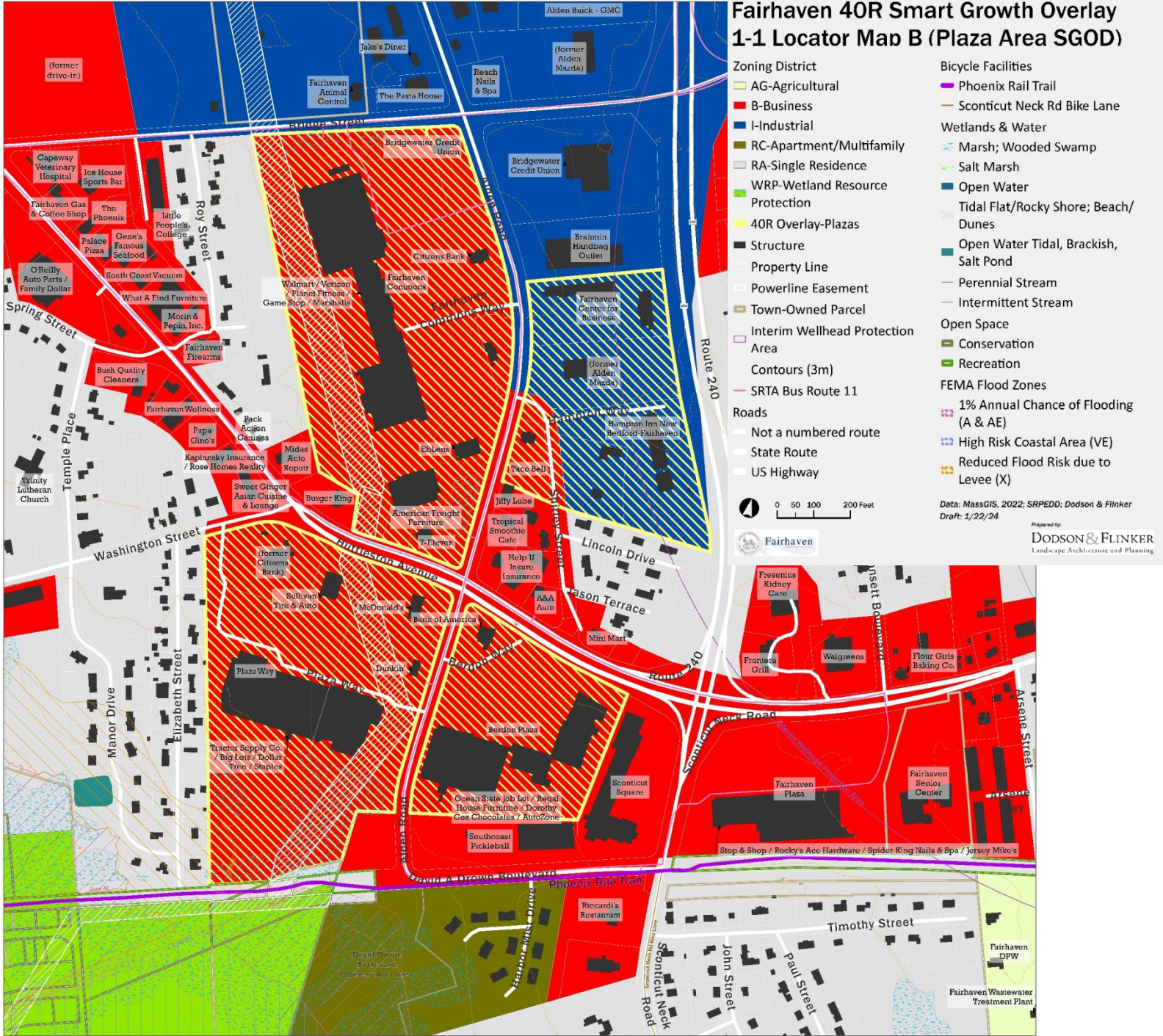
New





# Existing Zoning & Overlay

## Plazas Area



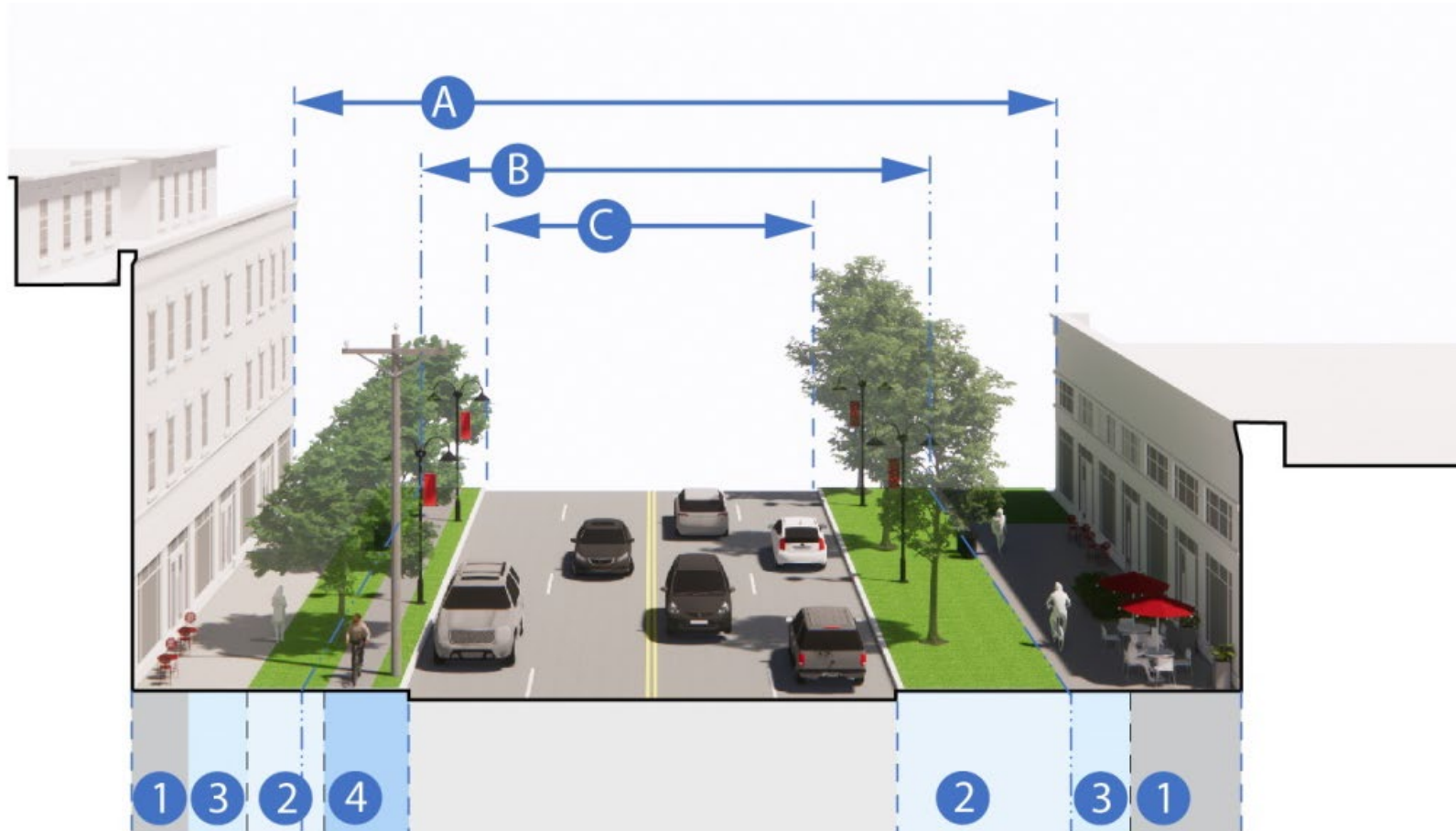


# Proposed Zoning Map



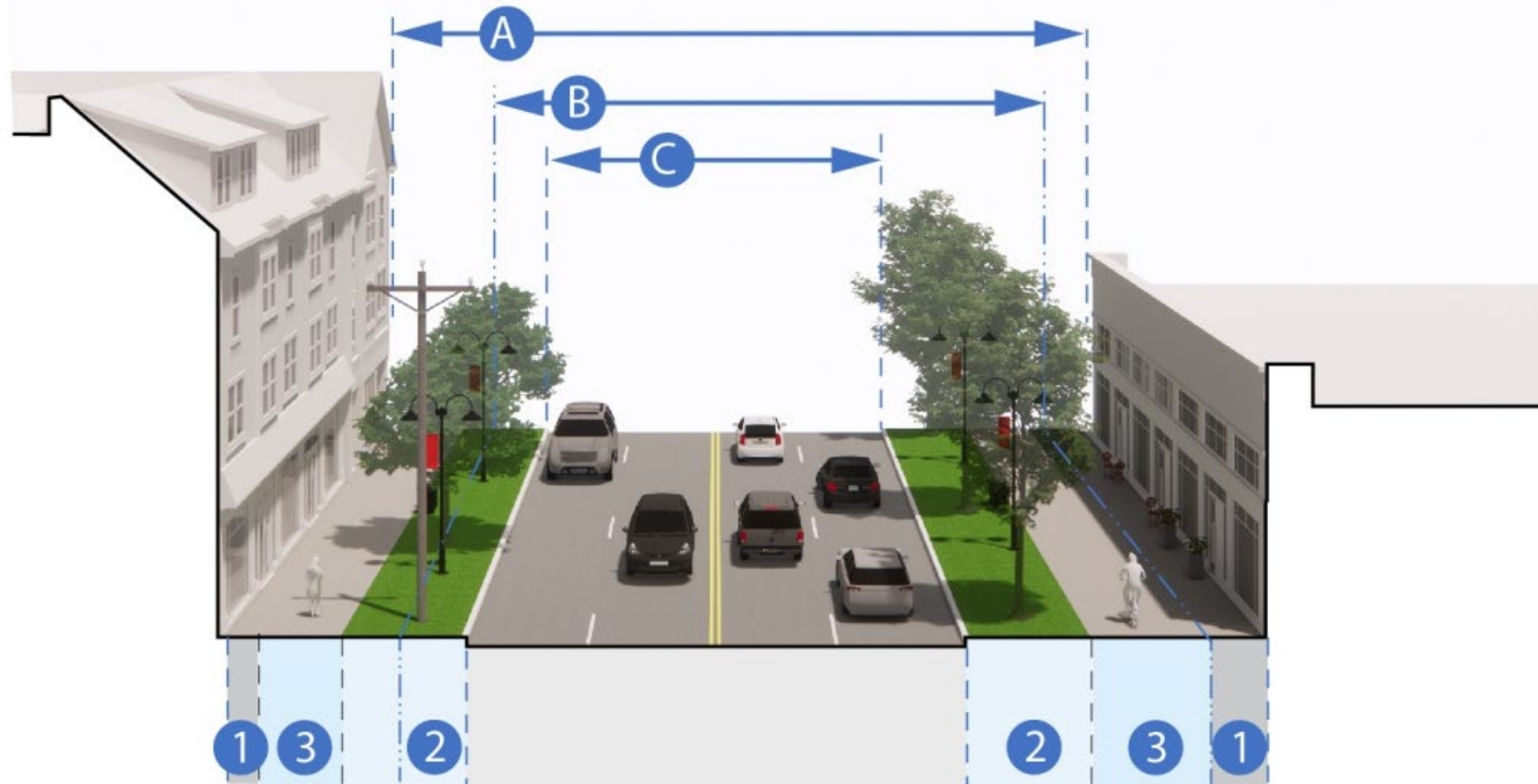


# Typical Alden Road (North End) Cross Section



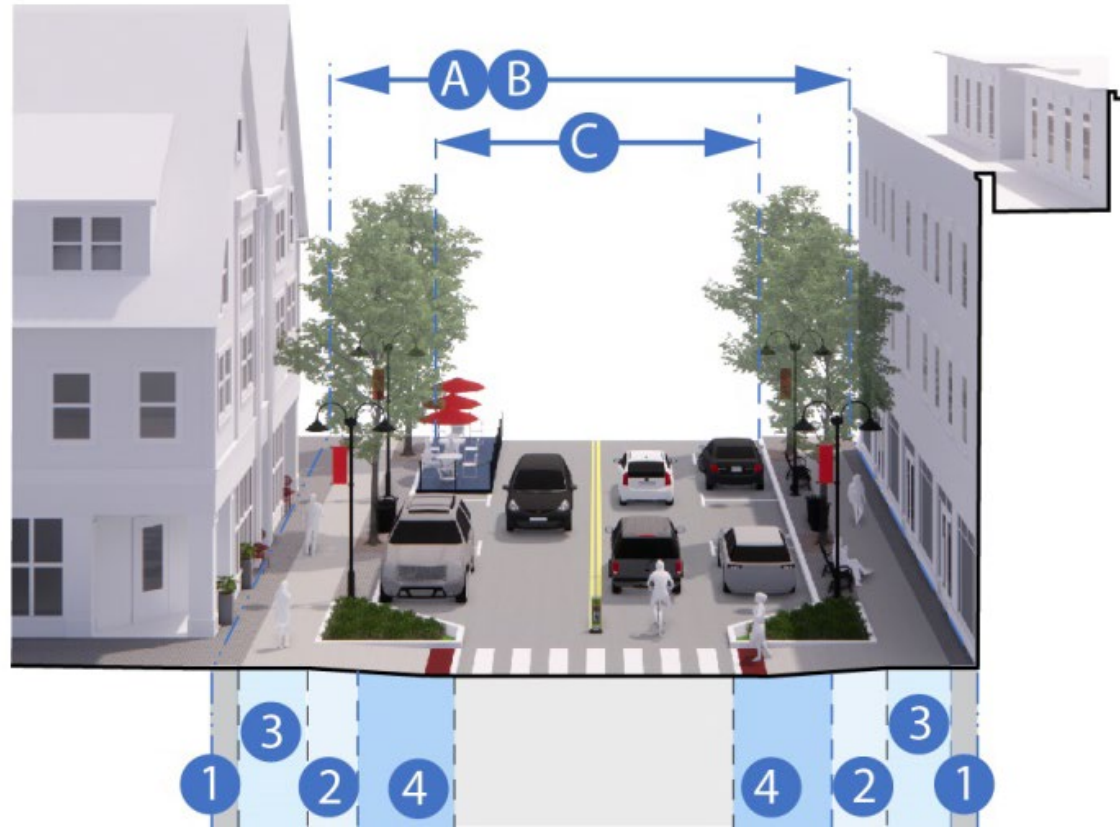


# Typical Alden Road (South End) Cross Section



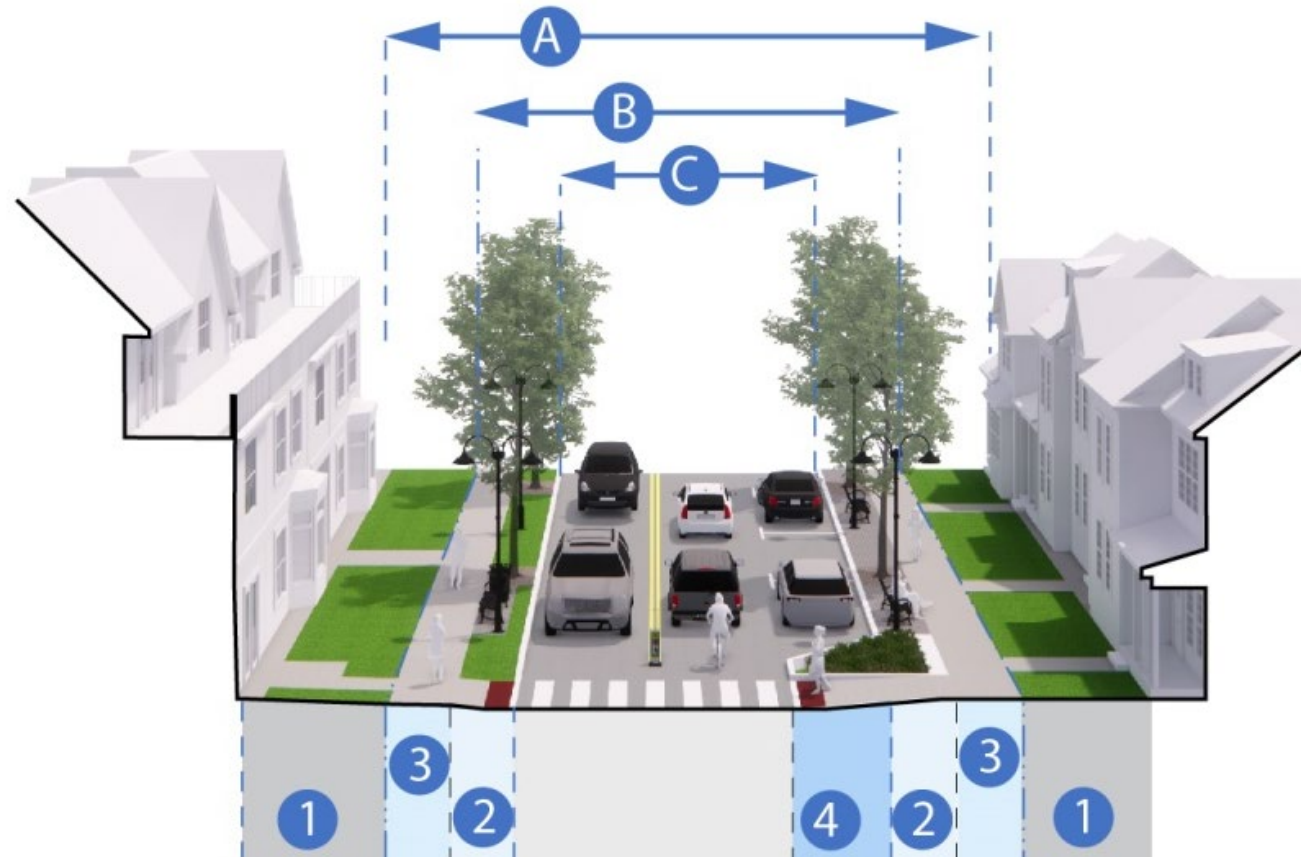


# Commercial Street Cross Section





# Neighborhood Street Cross Section





# Plazas Area-Dimensional Standards

<b>Lot size</b>	--
<b>Frontage</b>	20'
<b>Front setback</b>	Min/max by street type
<b>Side setback</b>	10' (0' for attached buildings)
<b>Rear setback</b>	10'
<b>Open space (minimum)</b>	10%
<b>Outdoor Amenity Space</b>	Minimum of 24 sq ft per dwelling unit if directly accessible from a habitable room in the unit; Minimum of 100 sq ft per dwelling unit if shared. [Note: can be counted toward open space]
<b>Civic Space</b>	If there are more than 50 dwelling units on a development site, then a minimum of 50 sq. ft. of civic space must be provided per unit.
<b>Building coverage (max)</b>	75%
<b>Building Height</b>	Min: 22' Maximum height per subdistrict: <ul style="list-style-type: none"><li>• Plazas A: max. 4 stories (55')</li><li>• Plazas B: max. 5 stories (65')</li><li>• Plazas C: max. 6 stories (75')</li><li>• Height also limited by District Transitional Buffer and Street Enclosure Standard</li></ul>



# Plazas Area-Dimensional Standards

## District Transitional Buffer <sup>1</sup>

- Applies to: areas designated on the zoning map
- Setback (A): 50' measured from the district boundary
- Baseline Building Height (B): 50' (also height limitations by subdistrict)
- Stepback for additional stories: Additional stories above the Baseline Building Height must be stepped back at 1:1 ratio (height:stepback).

## Upper story stepback at front of lot <sup>2</sup>

Per Street Type

## Story Height, Ground Floor

12' min

## Story Height, Upper Story

10' min

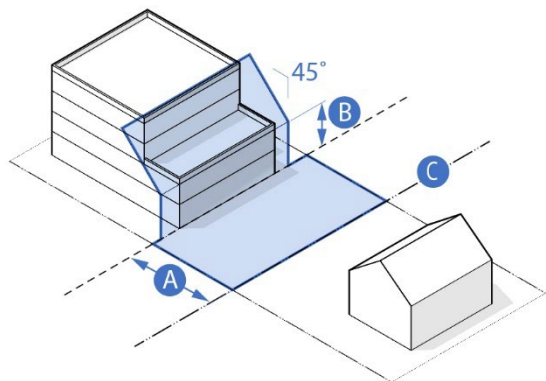
## Building frontage occupancy (min)

50%

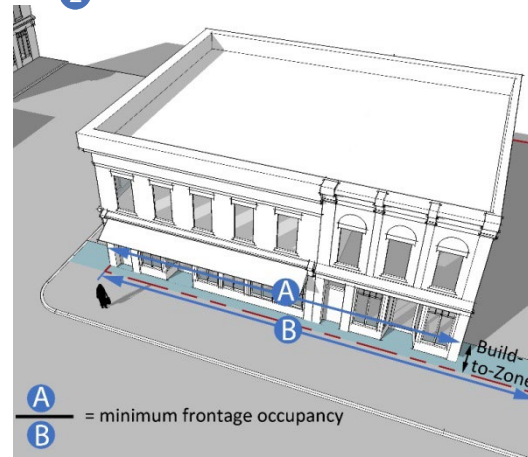
## Ground Floor Use Limitation Area <sup>3</sup>

30' min

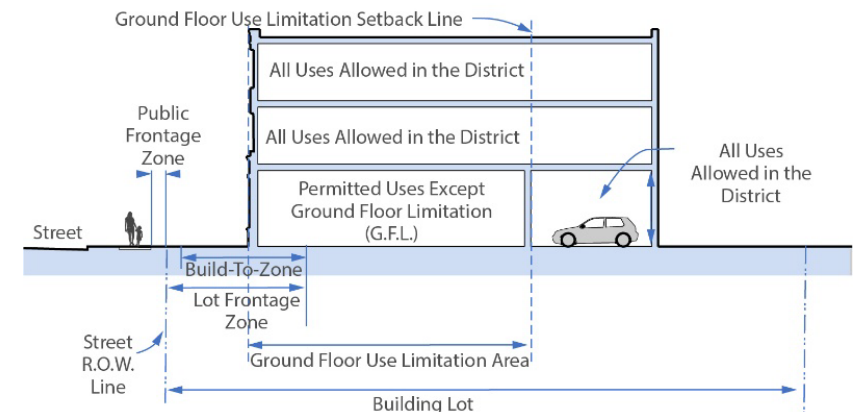
1



2



3





# Plazas Area-Dimensional Standards

## Density

20 units/acre max by right

Up to 35 units/acre with incentives by waiver

- Additional affordable housing units
- Deeper affordability level than that which is required
- Publicly accessible outdoor amenity space
- Improvements to offsite town-owned open space
- Complete streets improvements beyond those which are required

## Parking Requirements

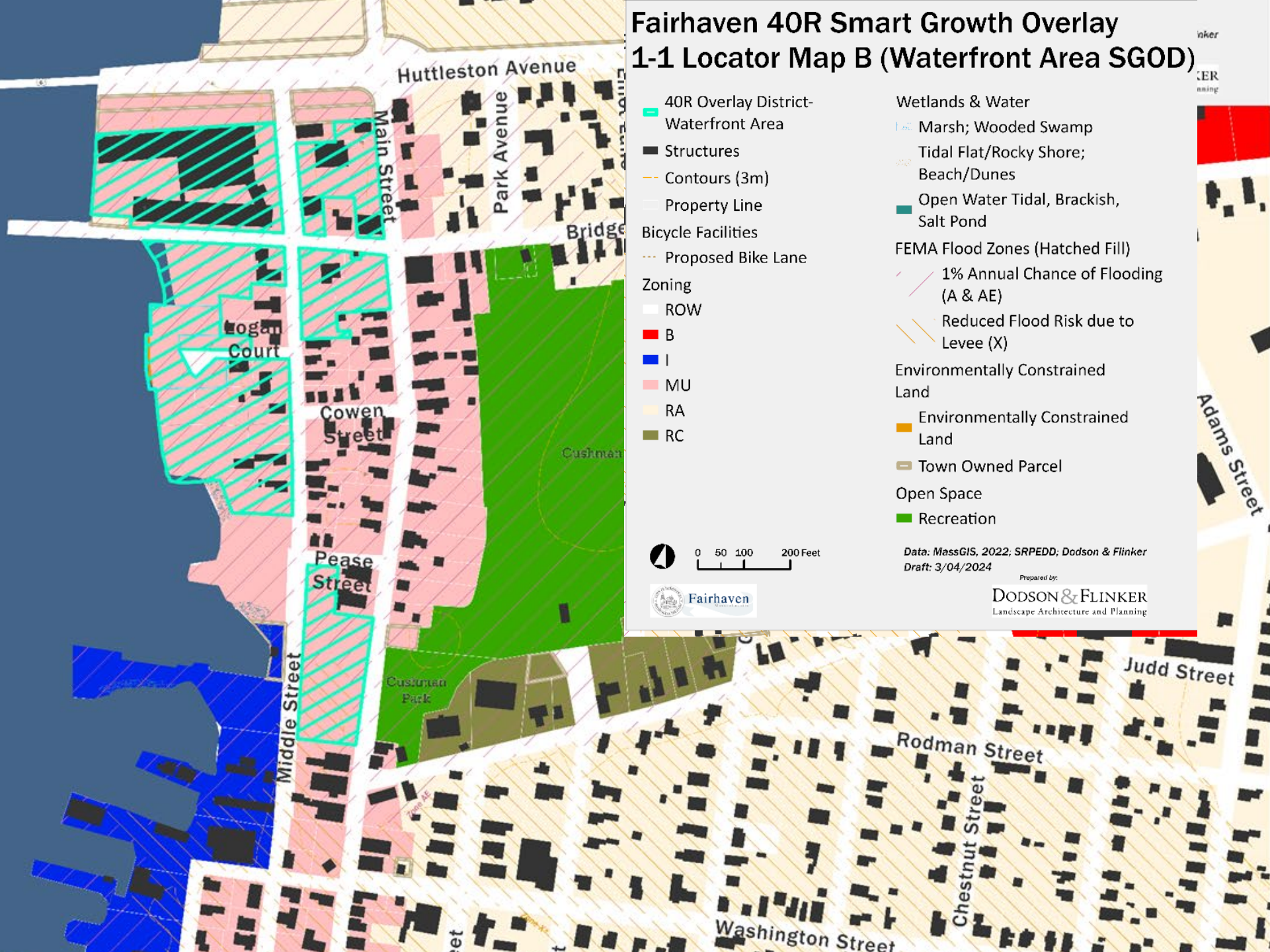
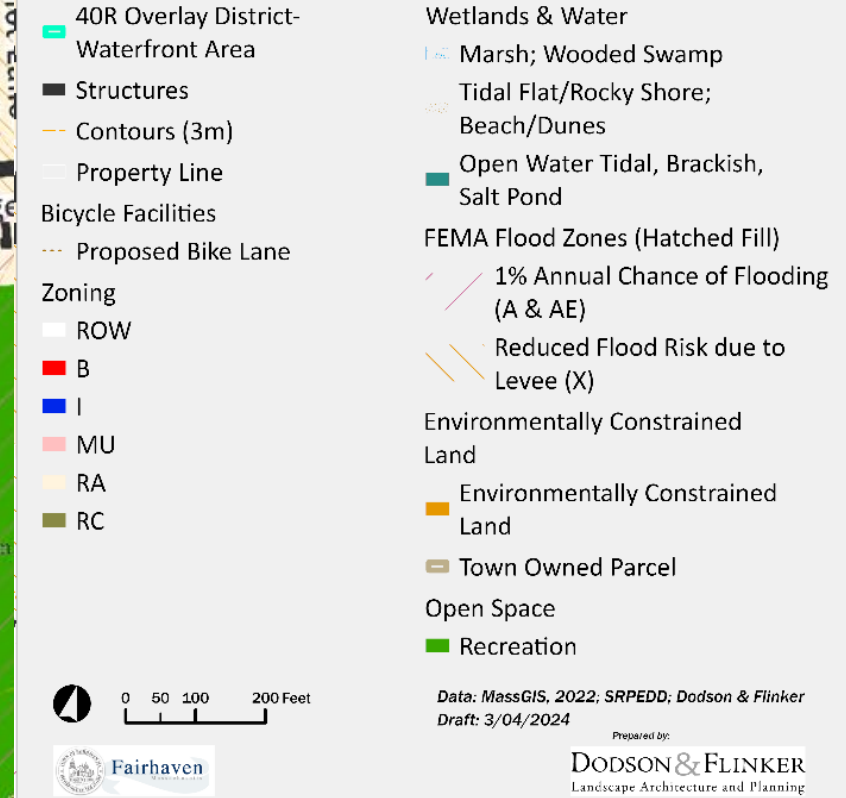
Per ITE Parking Generation Manual



# Existing Zoning & Overlay

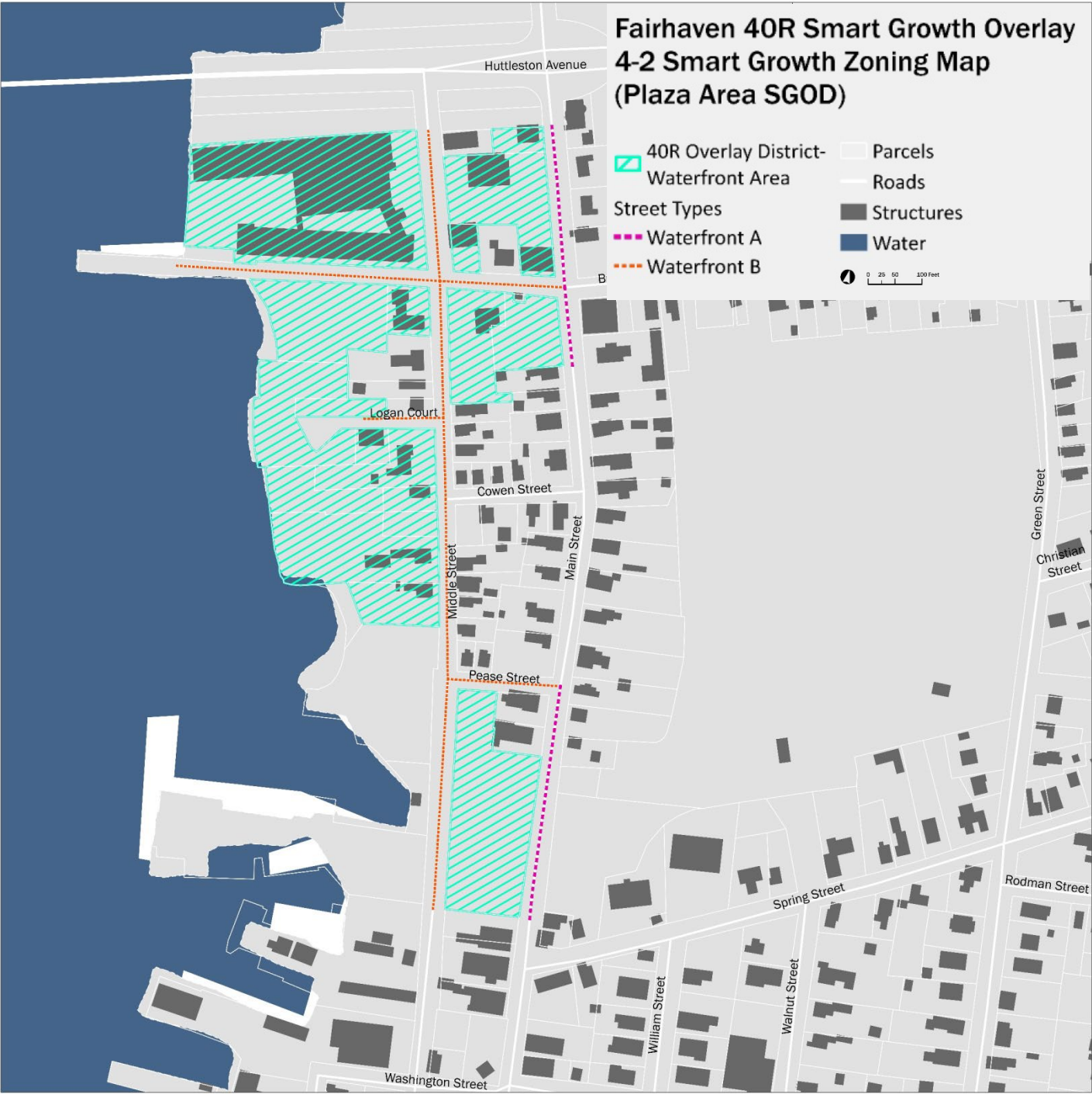
## Waterfront Area

### Fairhaven 40R Smart Growth Overlay 1-1 Locator Map B (Waterfront Area SGOD)





# Proposed Zoning Map





# Waterfront Area-Dimensional Standards

Lot size	--
Frontage	20'
Front setback	Min/max by street type (5' min/15' max)
Side setback	10' (0' for attached buildings)
Rear setback	20'
Open space (minimum)	10%
Outdoor Amenity Space	Minimum of 24 sq ft per dwelling unit if directly accessible from a habitable room in the unit; Minimum of 100 sq ft per dwelling unit if shared. [Note: can be counted toward open space]
Civic Space	If more than 25 dwelling units on a development site, then a minimum of 175 sq. ft. of civic space must be provided per unit.
Building coverage (max)	75%
Building Height	Min: N/A Max: 4 stories (45' ) <ul style="list-style-type: none"><li>• Height measured from highest flood elevation on a lot per 2070 1% ACEP per <a href="#">Massachusetts Coast Flood Risk Model (MC-FRM)</a></li><li>• Additional height limitations per District Transitional Buffer and Street Enclosure Standard</li></ul>



# Waterfront Area-Dimensional Standards

<b>District Transitional Buffer</b>	<ul style="list-style-type: none"><li>• Applies to: development adjacent to an existing parcel with an existing residential use that is not included in the 40R SGOD</li><li>• Setback: same as to applicable side or rear setback</li><li>• Baseline Building Height: 3 stories or 35 feet at the relevant side or rear setback.</li><li>• Stepback for additional building height: Additional stories above the Baseline Building Height must be stepped back at 1:1 ratio (height:stepback).</li></ul>
<b>Upper story stepback at front of lot</b>	<ul style="list-style-type: none"><li>• Per Street Type</li></ul>
<b>Story Height, Ground Floor</b>	10' min
<b>Story Height, Upper Story</b>	10' min
<b>Building frontage occupancy (min)</b>	50% When a lot fronts on both Middle Street and Main Street, the building frontage occupancy requirement applies only to the Main Street frontage.



# Waterfront Area-Dimensional Standards

Ground Floor Use Limitation Area	30' min
Density	<p>35 units/acre max by right</p> <p>Up to 60 units/acre with incentives by waiver. Waivers for additional density <u>may</u> be granted for:</p> <ul style="list-style-type: none"><li>• Additional affordable housing units</li><li>• Deeper affordability level than that which is required</li><li>• Publicly accessible outdoor amenity space</li><li>• Improvements to offsite town-owned open space</li><li>• Complete streets improvements beyond those which are</li></ul>
Parking Requirements	Per ITE Parking Generation Manual



## Contents:

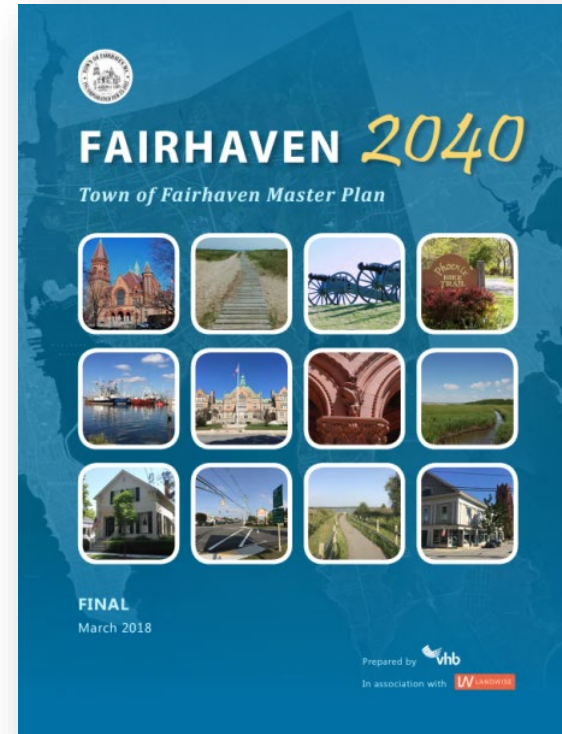
1. Introduction
2. Purpose
3. Applicability
4. Definition of Terms
5. Guiding Principles
  - Reflecting Fairhaven's Planning Goals and Objectives
  - Enhancing Fairhaven's Visual Character and Quality of Life
  - Organized around the shared space of the public realm
  - Supporting for Environmental, Economic and Social Sustainability
6. Design Standards for Neighborhoods, Blocks and, Streetscape
7. Vehicular Access and Parking Standards
8. Architectural Standards
9. Open Space and Landscape Standards
10. Lighting Standards
11. Signage Standards
12. Stormwater Management
- Appendix 1: Street Types



## 5. Guiding Principles

### 1. Reflects Fairhaven's vision for the area

- Supports implementation of the Fairhaven Master Plan
- Advances the Vision for the district as described in the conceptual plans.





## 5. Guiding Principles

### 2. Supports Town's goals for Visual Character and Quality of Life





## 5. Guiding Principles

### 3. Organized around the shared space of the public realm

- A unified network of walkable streets, greenways and public spaces
- Connected to surrounding streets and neighborhoods
- A clear, legible hierarchy of street types
- Buildings relate to and reinforce the streetscape





### 4. Supports Social, Economic and Environmental Sustainability

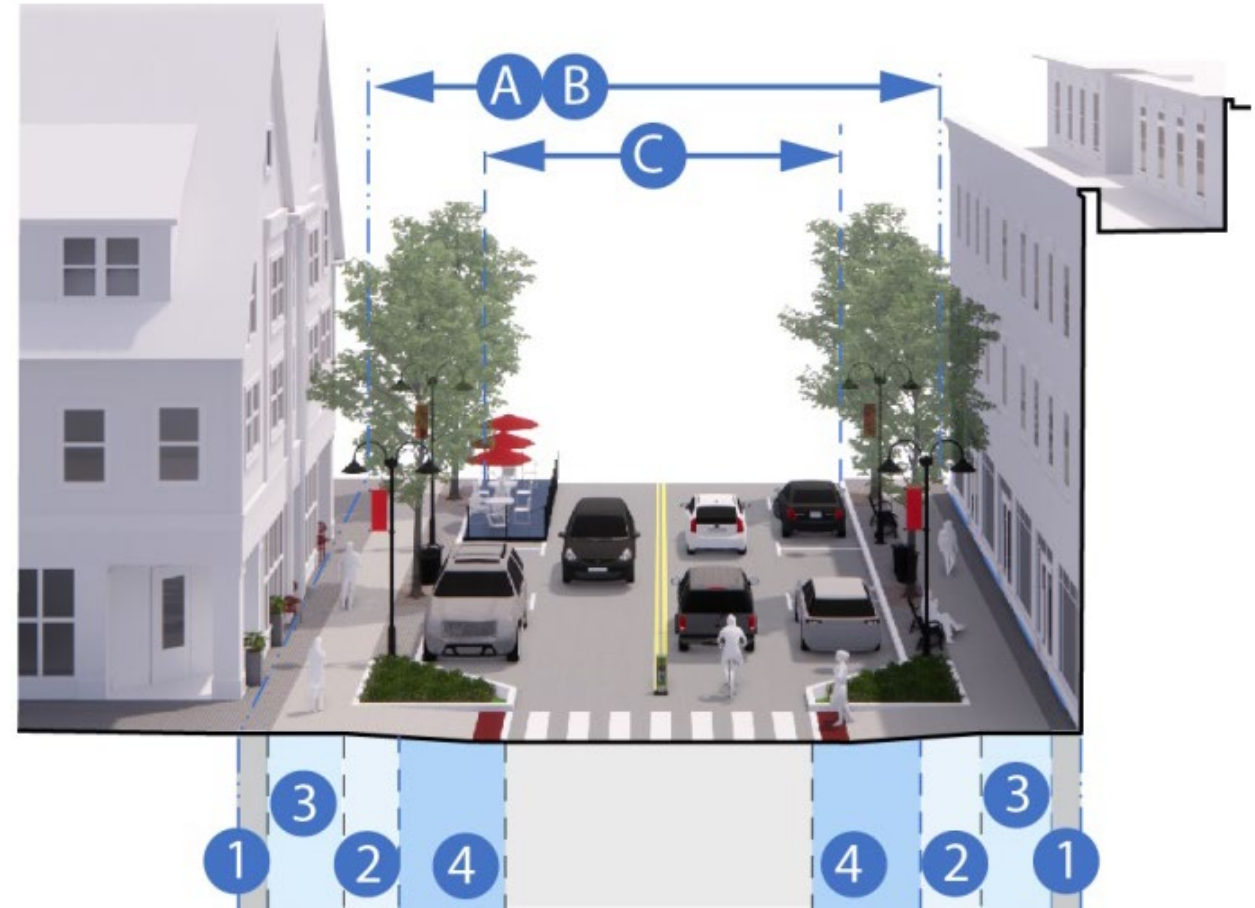
- Protects and enhances the natural environment
- Mixed uses within a compact, efficient footprint
- Supports diverse transportation choices
- Supports diverse housing choices
- Energy-efficient design and materials
- Meets LEED or other environmental performance standards
- Uses Low Impact Development/Green Stormwater management





## 6. Design Standards for the Streetscape

- Design Principles
- Block length
- Building Placement and Orientation
- Street Enclosure
- Street Design: new and upgraded streets
- Design & Materials for Sidewalks and Pedestrian Areas
- On-Street Parking
- Accessibility
- Street Furnishings
- Walls, Fences, and Hedges Along Lot Frontage Screening Elements: Walls, Fences and Hedges





## 7. Vehicular Access and Parking

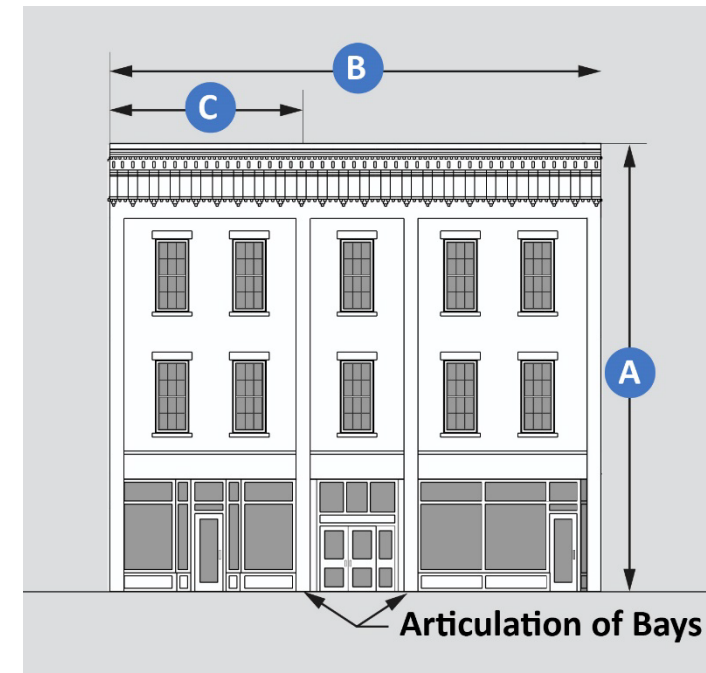
- Alleys and Access Driveways
- Connections Between Parking Areas
- Location, entrances, and access to surface parking and garages
- Loading Facilities
- Parking Materials
- Low Impact Development
- Structured Parking
- Shared Parking
- Off-site Parking
- Street-side Parking on Private Property
- Accessible Parking
- Bicycle Parking





## 8. Architecture

- Design Principles
- Siting of Structures
- Overall Building Shape, Massing, and Proportions
- Building Facades
- Building Height and Scale
- Roofs
- Design and Orientation of Entrances
- Windows and Doors
- Materials & Surface Appearance
- Porches, Awnings and Canopies
- Secondary Elements: Towers, Cupolas, Chimneys
- Service Areas, Mechanical Systems, HVAC Equipment, Utilities





## 9. Open Space and Landscaping

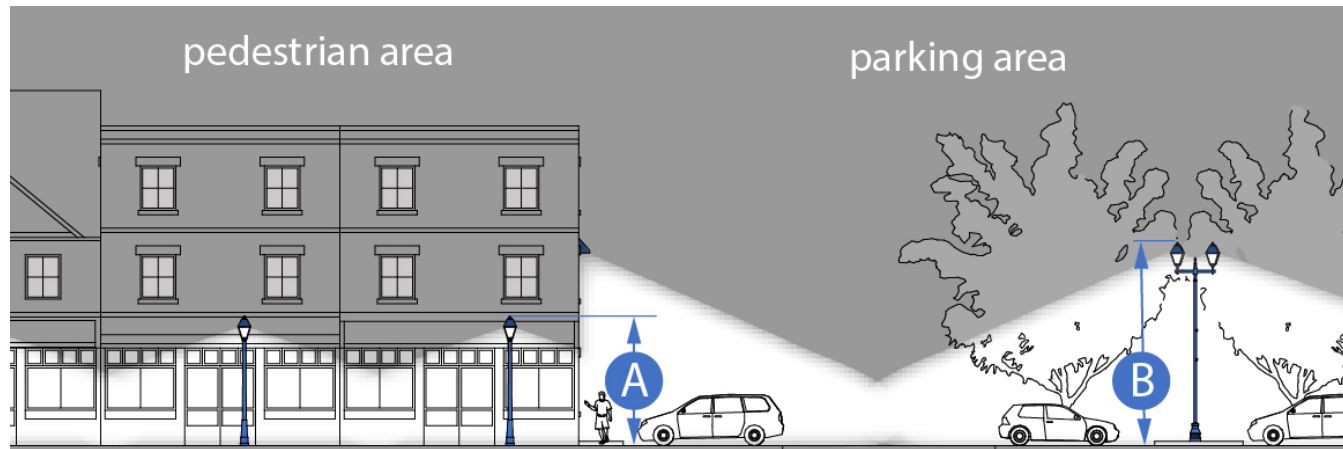
- Design Principles
- Significant Tree Protection and Preservation
- Soil
- Parking Lot Landscaping
- Street Trees
- Site Landscaping
- Buffer and Screening Plantings
- District Transitional Buffer





## 10. Lighting

- Design Principles
- Light Source, Light Intensity, and Control of Glare
- Height of Fixtures
- Hours of Operation
- Streetlights, Parking Lot Lighting, Building Lighting



Even Light Distribution



# 11. Signage

- Design Principles
- Size
- Materials
- Lighting
- Wall Signs
- Window Signs
- Projecting Signs
- Awnings, Canopies and Marquees



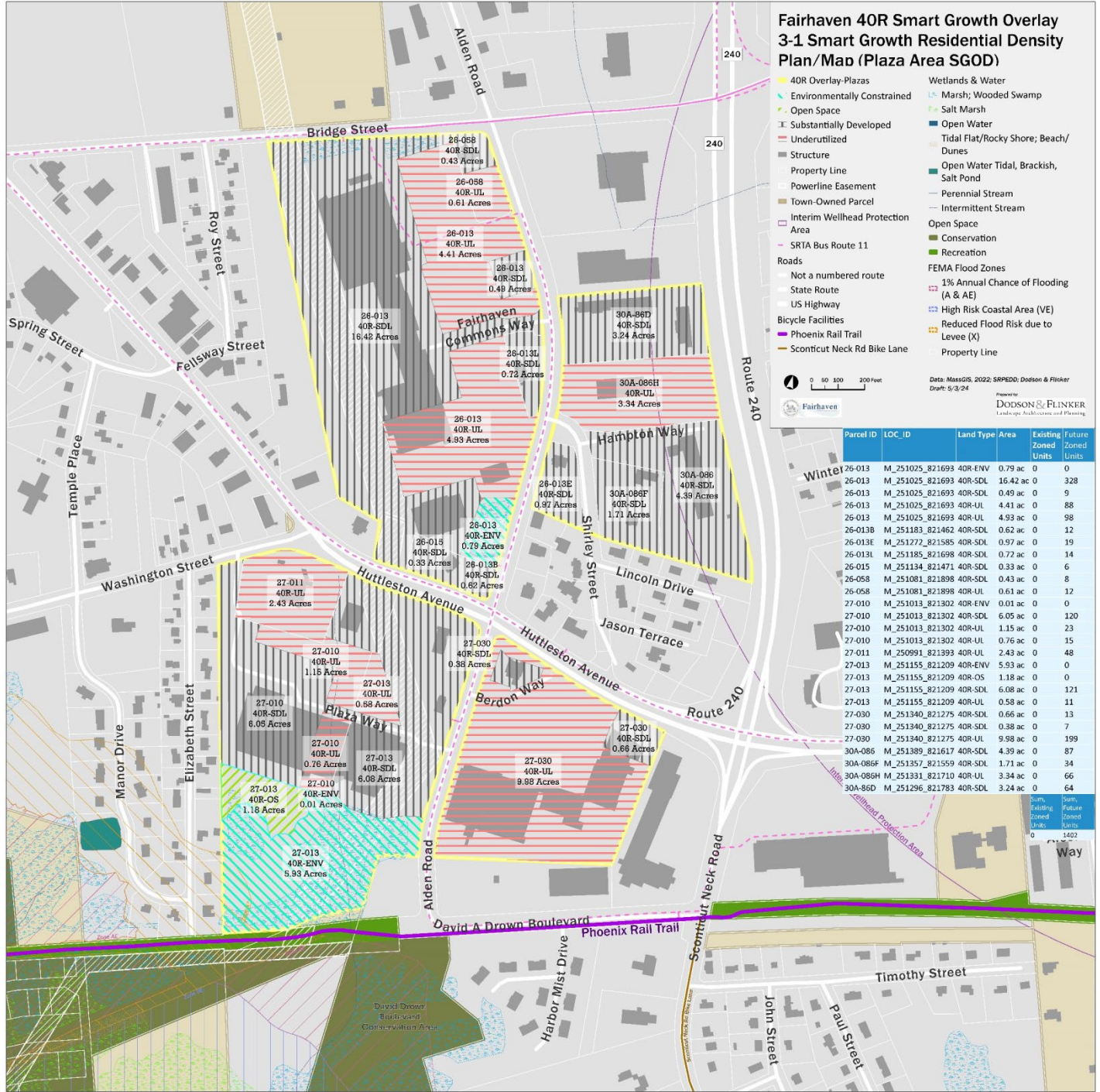


# EOHLC Application Map—Estimating Incentive Payment

## Plazas Area

**Size of Proposed District: 79 acres**  
**Number of Incentive Units: 286**  
**Potential Bonus Payment Units: 1,405\***

\*This is the zoning capacity. In reality, zoning capacity is rarely fully built for a variety of reasons including: it is not profitable to redevelop sites with valuable existing buildings and businesses; site conditions limit what can actually be built; and local real estate markets cannot absorb large amounts of new housing and commercial spaces. Zoning a larger area creates a range of short-, medium, and long-term opportunities for redevelopment that will help implement a variety of town goals.

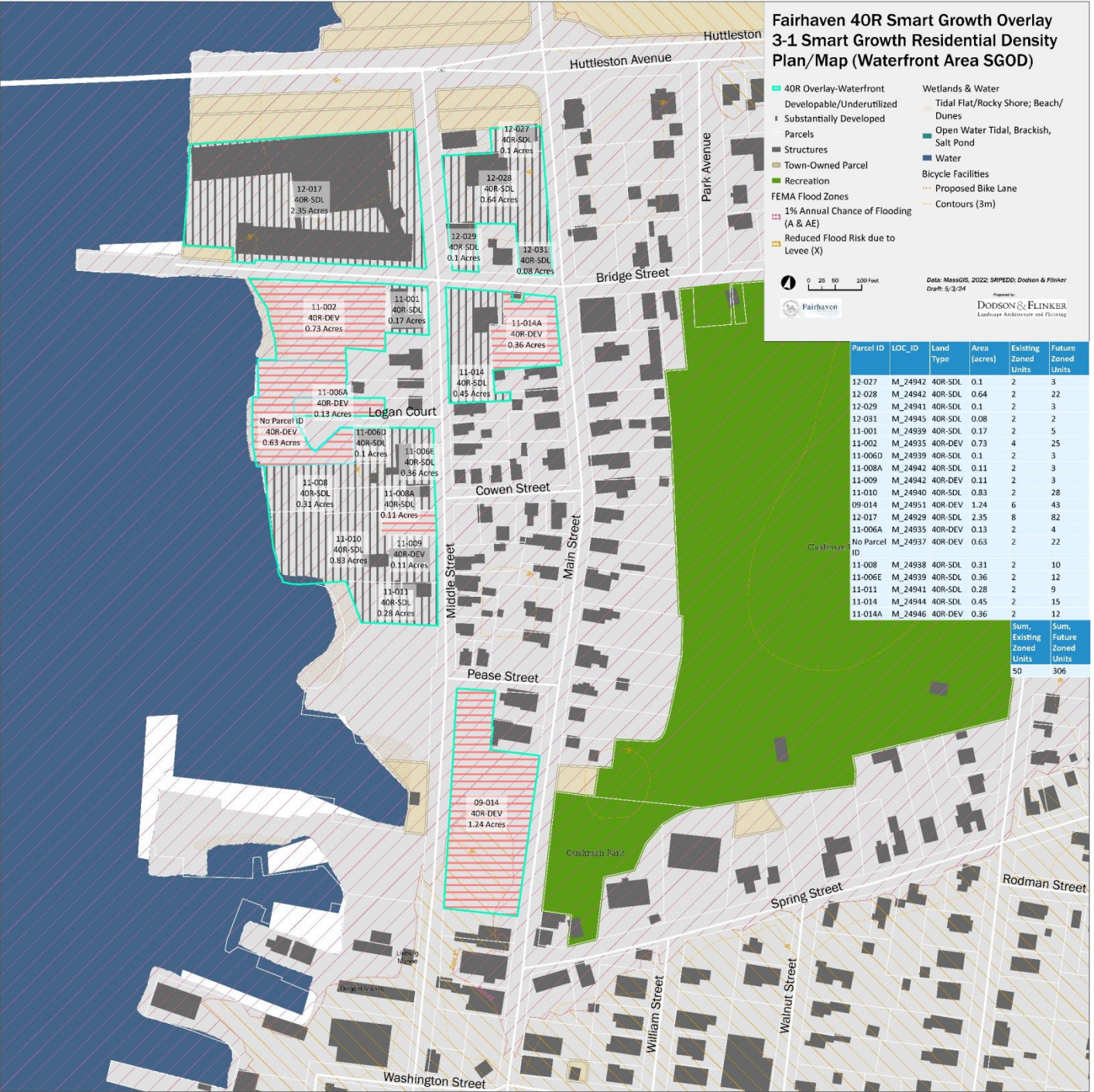




# EOHLC Application Map—Estimating Incentive Payment Waterfront Area

**Size of Proposed District: 9 acres**  
**Number of Incentive Units: 46**  
**Potential Bonus Payment Units: 256\***

\*This is the zoning capacity. In reality, zoning capacity is rarely fully built for a variety of reasons including: it is not profitable to redevelop sites with valuable existing buildings and businesses; site conditions limit what can actually be built; and local real estate markets cannot absorb large amounts of new housing and commercial spaces. Zoning a larger area creates a range of short-, medium, and long-term opportunities for redevelopment that will help implement a variety of town goals.





## Estimated 40R Payments

**Total Incentive Payment for both areas: \$350,000**  
**Bonus Payments: \$3,000 per unit built**

Plus, Chapter 40S reimbursement for the net cost of educating students living in new housing in smart growth districts



The vision plans for the Waterfront Area (left) and Plazas Area (below) are at the same scale—showing how much larger the Plazas Area is than the Waterfront Area.

